



District Department of Transportation

2015 High Crash Intersection Site Visits

2016 End of Year Update

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GOVERNMENT OF THE
DISTRICT OF COLUMBIA
MURIEL BOWSER, MAYOR

High Crash Intersection Site Visits

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Table of Contents

Introduction	4
First Street and Massachusetts Avenue NE	5
Bladensburg Road and New York Avenue NE	7
14 th Street and U Street NW	9
Minnesota Avenue and Benning Road NE	11
M Street and Wisconsin Avenue NW.....	13
Conclusion.....	14

List of Tables

Table 1: Site Visit Locations and Dates	4
Table 2: First Street and Massachusetts Avenue NE Next Steps	7
Table 3: Bladensburg Road and New York Avenue NE Next Steps	8
Table 4: 14th Street and U Street NW Next Steps	10
Table 5: Benning Road and Minnesota Avenue NE Next Steps	12
Table 6: M Street and Wisconsin Avenue NW Next Steps.....	14

List of Figures

Figure 1: First Street and Massachusetts Avenue NE Site Visit Observations	5
Figure 2: New York and Bladensburg Road NE Site Visit Observations	7
Figure 3: 14th and U Street NW Site Visit Observations.....	9
Figure 4: Minnesota Avenue and Benning Road NE Site Visit Observations	11
Figure 5: M Street and Wisconsin Avenue NW Site Visit Observations.....	13

Introduction

The District Department of Transportation conducted site visits to five high crash intersections in Summer 2015 with Councilmember Mary Cheh. Following these site visits, DDOT designated a staff location lead to each intersection, along with issue leads to address specific mode categories (pedestrian, bicycle, and vehicle). DDOT determined resolutions and assigned implementation dates for each intersection issue. Department staff hold regular meetings with internal working groups and senior management to facilitate progress, and coordinate regularly with external partners including the Metropolitan Police Department, the Department of For-Hire Vehicles, and Metro. To date, of the 46 issues identified across all five intersections, DDOT has resolved 34, eight are pending action as part of other capital projects, and four rely on long-term solutions.

Table 1: Intersection Issues Status

Site Visit Location	Issues Resolved	Issues Pending Action	Long-Term Issues
First Street and Massachusetts Avenue NE	11	0	1
Bladensburg Road and New York Avenue NE	6	2	1
14 th Street and U Street NW	8	1	0
Minnesota Avenue and Benning Road NE	5	3	1
Wisconsin Avenue and M Street NW	4	2	1

The following update details work completed on each intersection to date, expected issue completion dates, and next steps.

First Street and Massachusetts Avenue NE

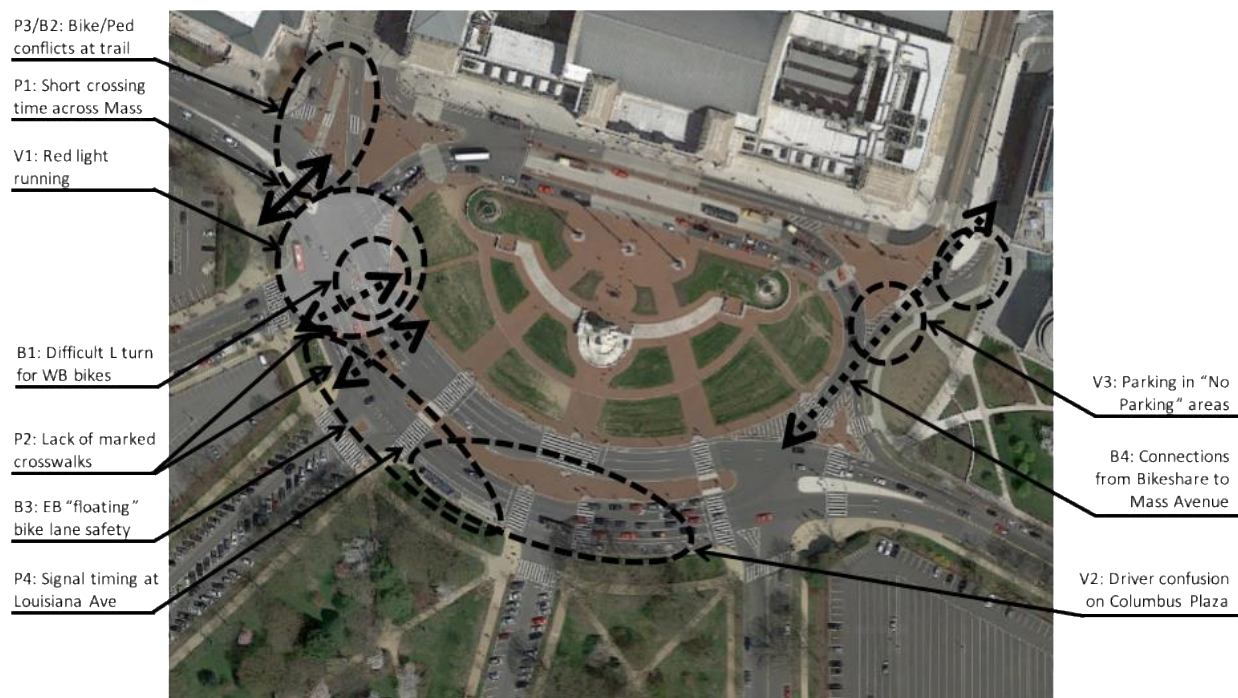


Figure 1: First Street and Massachusetts Avenue NE Site Visit Observations

1.1. Next Steps

Issue	Resolution	Status	Timeline
Pedestrian Issues			
P1. Short crossing time for pedestrians across Mass Ave NE.	Additional time can be added, requiring re-optimization of Columbus Circle signals and shifting to 100 second cycles after peak hours	Work completed	Implemented 3/31/16
P2. Lack of marked crosswalks across Massachusetts Avenue.	Field investigation identified two potential options. Both would require installation of ADA ramps, modification of the median, and pedestrian signals, relocation of 4 catch basins, a traffic signal pole and light poles. There are also operational concerns with the signal phasing, turn lanes, and vehicular storage capacity along Columbus Circle due to several closely spaced signals.	Additional design and cost estimation would be mid-term activity potentially coordinated with other physical improvements. No work programmed at this stage.	Resolved 6/3/16

Issue	Resolution	Status	Timeline
P3. Pedestrian/Bike conflicts from 1st Street/MBT to Mass Ave.	Add green paint and refresh bike symbols in concrete bike area, chevrons to enhance bike lanes south and north of crosswalk by bike station, bike signal signs under bike signals; remove ped crossing heads in bike lane; add shared lane markings in last segment of 1st St before Mass Ave.	Work completed	Implemented 5/31/16
P4. Pedestrian phasing at Louisiana and Mass. Ave.	Evaluate signal phasing to reduce conflicts with pedestrians.	Work completed	Implemented 5/31/16
Bike Issues			
B1. Difficulty of westbound bikes on Mass Ave.	Add bike boxes on westbound Mass. Ave. at Louisiana and pocket lane at E St	Work completed	Implemented 5/31/16
B2. Pedestrian/Bike conflicts from 1st Street/MBT to Mass Ave.	Add green paint and refresh bike symbols in concrete bike area, chevrons to enhance bike lanes south and north of crosswalk by bike station, bike signal signs under bike signals; remove pedestrian crossing heads in bike lane; add shared lane markings in last segment of 1st St before Massachusetts Avenue.	Work completed	Implemented 5/31/16
B2. Pedestrian/Bike conflicts from 1st Street/MBT to Mass Ave.	Widen pedestrian curb ramp on the southwest corner of 1 st St, E St, and Massachusetts Avenue.; install bike crosswalk by separating from pedestrian crosswalk.	Sidewalks under jurisdiction of Architect of the Capitol. Potential to modify ramps in conjunction with other bike improvements.	Implementation timeline to be developed with AOC.
B3. Eastbound Mass. Ave. biking conditions.	Add bike box on eastbound Mass. Ave. at 1st; add right turn only pavement markings and RIGHT LANE MUST TURN RIGHT signs on westbound Mass. Ave. at Louisiana and Delaware.	Work completed	Implemented 6/13/16
B4. Bike connection from bike-share station on 2nd Street to Mass. Ave.	Extend contra-flow south of bike station to crosswalk. At that point use wayfinding to the existing sidewalk network through plazas.	Preliminary contra-flow design complete. Behavioral observations indicate multiple desire lines and potential conflict areas. Not recommended for installation.	Resolved 11/1/16

Issue	Resolution	Status	Timeline
Vehicle Issues			
V1. Red light running.	Explore automated red light enforcement with MPD.	MPD determined that Camera is not possible on Columbus Circle curve.	Resolved 4/8/16
V2. Driver confusion on Columbus Circle.	Add lane configuration pavement markings sign; add “puppy tracks” on Mass. Ave. through both legs of 1 st St.	Through arrows and markings installed.	Implemented 6/13/16
V3. Parking in no-parking areas.	Explored the addition of flex posts or other barriers to keep drivers from parking in these areas.	Will continue to monitor for safety issues.	Resolved 5/16/16

Table 2: First Street and Massachusetts Avenue NE Next Steps

Bladensburg Road and New York Avenue NE

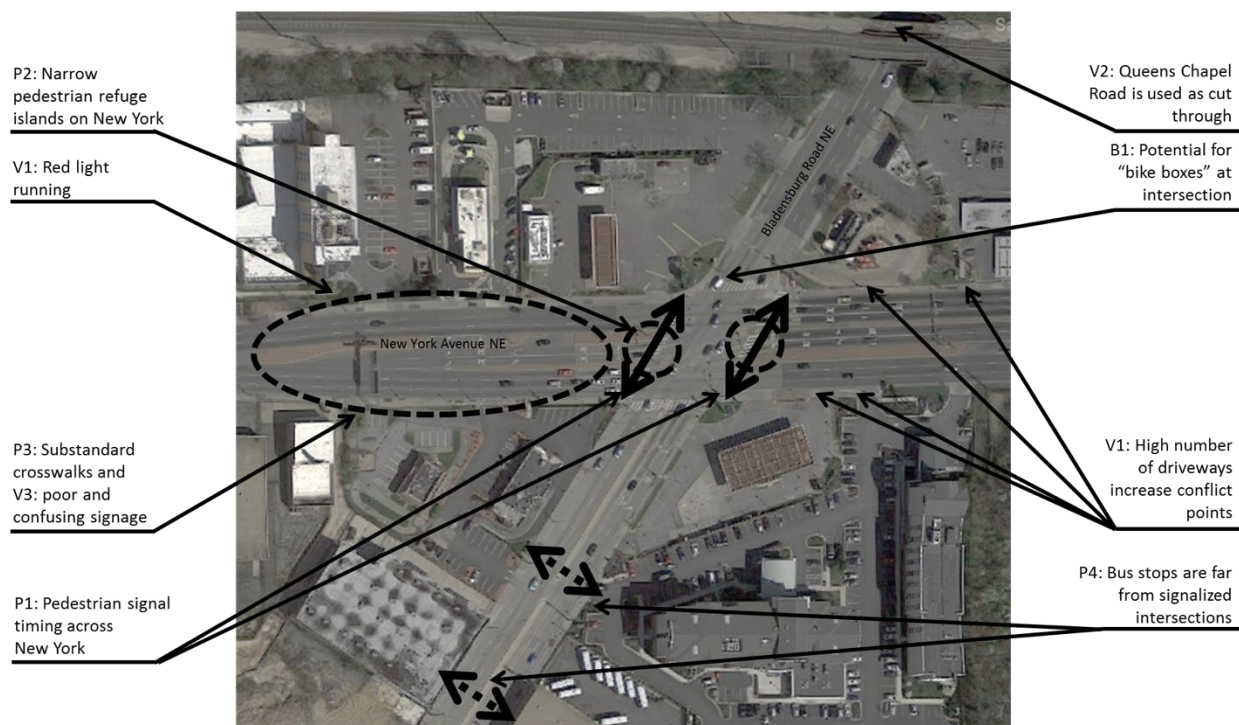


Figure 2: New York and Bladensburg Road NE Site Visit Observations

1.2. Next steps

Issue	Resolution	Status	Timeline
Pedestrian Issues			
P1. Pedestrian crossings of New York Avenue.	Switch phase from leading to lagging for westbound to southbound protected left and northbound to eastbound.	Work complete	Implemented 5/12/16
P2. Pedestrian refuge islands on New York.	Fixing curb ramps and medians as part of NHS restoration in FY17.	ADA evaluation was completed. Recommendations were forwarded to Asset Management for implementation by September 30, 2017.	Implementation to be scheduled in 2017.
P3. Pedestrian conditions along New York Avenue.	Fixing curb ramps and medians as part of NHS restoration in FY17.	ADA evaluation was completed. Recommendations were forwarded to Asset Management for implementation by September 30, 2017.	Implementation to be scheduled in 2017.
P4. Bladensburg Road south of New York Avenue.	Install rectangular rapid flash beacon.	Work completed.	Completed 9/15/16.
Bike Issues			
B1. Bike conditions at intersections.	Install bike box on northbound Bladensburg to ease left turns onto Queens Chapel Rd. Climbing lane on Queens Chapel from Bladensburg to 22 nd St.	Work complete	Implemented 6/18/16
B2. Bicycle racks at Fifth District Police Station.	Install bicycle racks.	Racks delivered to MPD for installation	Completed 7/31/2016.
Vehicle Issues			
V1. Access management and curb cuts.	Assess potential to close some driveways that do not meet DDOT standards. Evaluate potential closures as part of New York Avenue Streetscape project.	Anticipated completion of New York Avenue Streetscape study in late 2017.	Implementation TBD.
V2. Queens Chapel Road gets used as a cut-through when traffic is congested along New York Ave.	MPD already enforcing oversize vehicle restrictions in the vicinity.	MPD is currently enforcing in that vicinity.	Resolved 6/17/16
V3. Driver confusion and signage.	Upgrade lane use signage.	Work complete	Implemented 5/12/16

Table 3: Bladensburg Road and New York Avenue NE Next Steps

14th Street and U Street NW

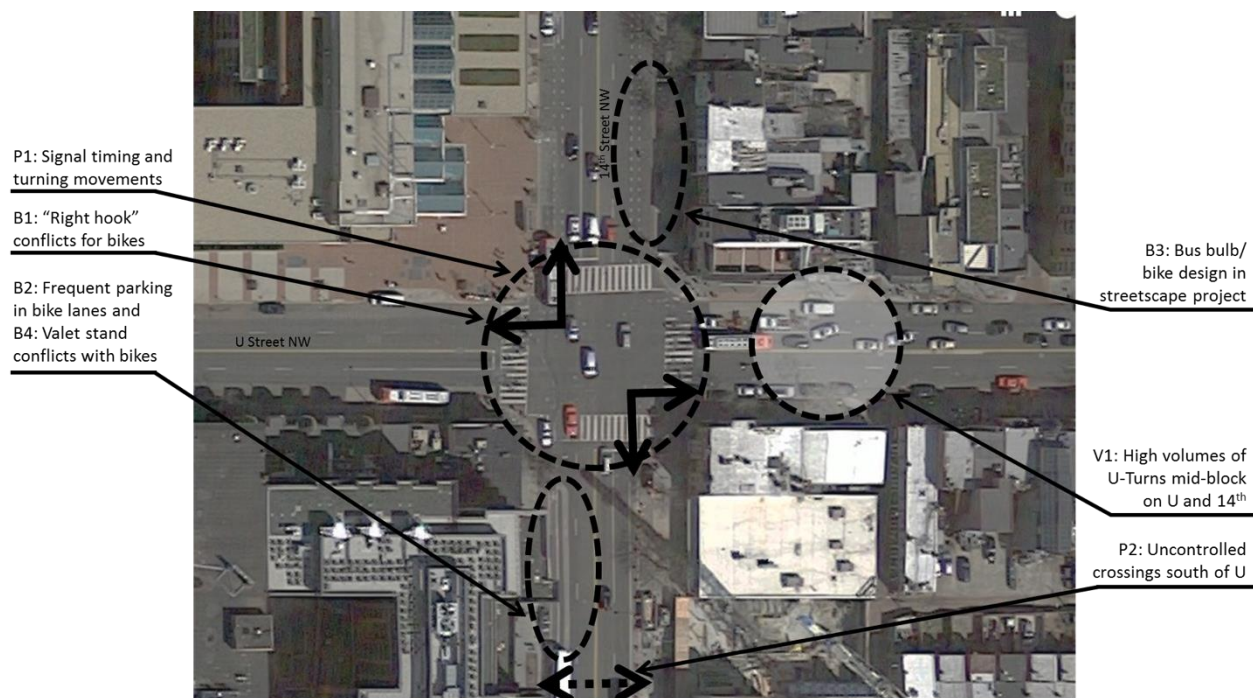


Figure 3: 14th and U Street NW Site Visit Observations

1.3. Next steps

Issue	Resolution	Status	Timeline
Pedestrian Issues			
P1. Pedestrian crossings and signal timing	A Barnes Dance is not possible given traffic levels overall. Elimination of signal phase will be conducted in two phases: Phase elimination without the removal of the signal head, followed by permanent removal.	New signal design removes leading left phasing and adds additional Flashing Don't Walk time.	Implemented 4/30/16

Issue	Resolution	Status	Timeline
P2. Pedestrian crossings on 14th Street corridor.	Locations are too close to adjacent signals to meet HAWK standards. Locations do not meet DDOT's RRFB guidelines.	RRFB evaluation complete. Crosswalk markings are in place.	Resolved 4/8/16
P3. Late night/weekend pedestrian volumes.	Current traffic control deployment as part of evening deployment shift until 10pm.	Deployment in place Wednesday-Sunday.	Implemented 4/8/16
Bike Issues			
B1. Turning movement conflicts for bikes.	Implement green paint prior and through the intersection.	Green paint installed.	Implemented 5/31/16
B2. Parking in bike lanes.	Work with DPW and MPD to improve enforcement of parking in bike lanes.	Flagged for DPW/MPD enforcement.	Implemented 4/8/16
B3. Potential conflicts between "bus bulbs" and bikes.	Include in final design for 14th Street streetscape project.	Concept design is complete and will be incorporated into final design for 14th Street project. Contract preparing to advertise for construction.	Implementation TBD based on construction.
B4. Valet stand conflicts with bike lanes.	Work with permit holders to improve compliance, including enforcement.	Monday-Sunday enforcement began in May.	Implemented 5/16/16
Vehicle Issues			
V1. U-Turns mid-block along U St and 14th St.	DC Taxicab Commission education.	DCTC conducted education campaign.	Implemented 4/22/16-4/23/16
V2. Speeding in corridor.	MPD has red light and speed on green camera in place for southbound 14th St and U St. Flagged for MPD enforcement in corridor with 3rd District.	Sent to MPD for evaluation.	Implemented 1/1/16

Table 4: 14th Street and U Street NW Next Steps

Minnesota Avenue and Benning Road NE

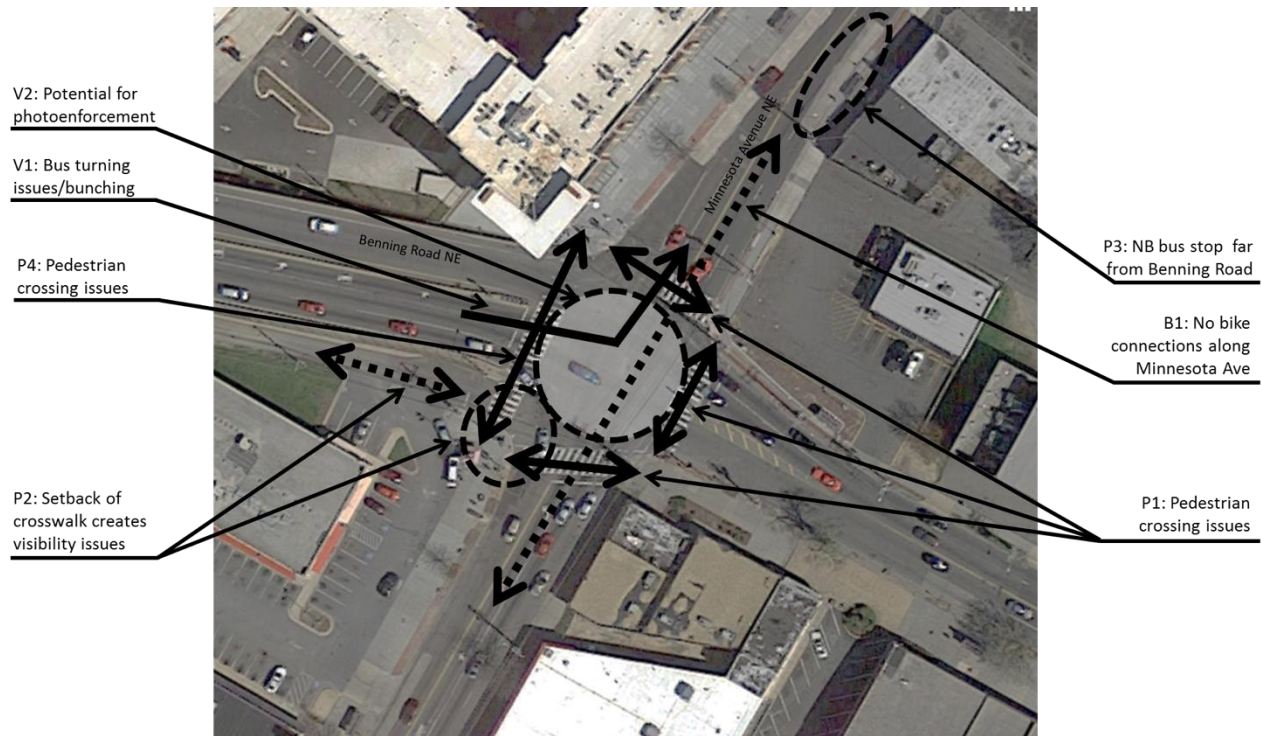


Figure 4: Minnesota Avenue and Benning Road NE Site Visit Observations

1.4. Next steps

Issue	Resolution	Status	Timeline
Pedestrian Issues			
P1. Pedestrian crossing improvements.	Refurbish crosswalks and install flexible posts.	Pedestrian crosswalks were refurbished. Flexible posts for temporary pedestrian refuge installed.	Implemented 4/22/16
P1. Pedestrian crossing improvements.	Redesign pedestrian crossing. Pedestrian crossing evaluation and design will be implemented under the Minnesota Avenue Phase II project.	Funding for project obligated and in procurement process.	Implementation date TBD based on construction schedule.
P2. Setback of south side Minnesota Avenue pedestrian crossing.	Improve visibility at the intersection. Evaluate diagonal pedestrian crosswalk design. Improved pedestrian crossing will be implemented during Phase II construction.	Funding for project obligated and in procurement process.	Implementation date TBD based on construction schedule.

Issue	Resolution	Status	Timeline
P3. Bus stop locations.	Coordinate with WMATA.	Short term bus stop relocation was discussed. WMATA is not planning to make any changes before Phase II construction.	Resolved 4/22/16
P3. Bus stop locations.	Relocate the bus stop close to the intersection.	Bus stop relocation evaluation and design is on-going and will be implemented in Minnesota Avenue Phase II construction.	Implementation date TBD based on construction schedule.
P4. Benning Road pedestrian crossing.	Evaluate signal timing. Improve visibility of pedestrian crosswalk. Pedestrian crossing times were optimized and updated as part of citywide Signal Optimization project.	No further timing changes are recommended currently.	Implemented 4/15/16
Bike Issues			
B1. Bike access on Minnesota	Evaluate Bike access on Minnesota Avenue. Encourage biking on Anacostia trail, with signage directing cyclists.	The northern section of Minnesota Ave from Meade to Eastern could accommodate bike lanes. Mid-term a lane could be reconfigured for bikes. The Anacostia Riverwalk Trail is the recommended parallel route. A long-term option is to acquire the CSX railway corridor for a trail.	Anacostia Riverwalk Trail opened 10/31/16.
Vehicle Issues			
V1. Signal operations for transit vehicles.	Evaluate phasing, especially for eastbound to northbound X2 bus.	Increase green time for eastbound to northbound left turn in PM rush. Rephasing is not possible with existing infrastructure constraints.	Implemented 4/15/16
V2. Driver behavior, including blocking the box and red light running.	Evaluate photo enforcement opportunities with MPD and/or TCO presence. Move "Do not block firehouse" sign closer to the station.	Block box enforcement technology is not available at this time. TCOs are working from 3 pm to 7 pm and safety technicians are assisting pedestrians from 10 am to 1 pm. "DO NOT BLOCK FIREHOUSE" sign and a stop bar were installed for short-term improvements. A new traffic signal will be installed during Phase II of the project.	Implemented 6/1/16. New signal implementation TBD based on streetscape construction schedule.

Table 5: Benning Road and Minnesota Avenue NE Next Steps

M Street and Wisconsin Avenue NW

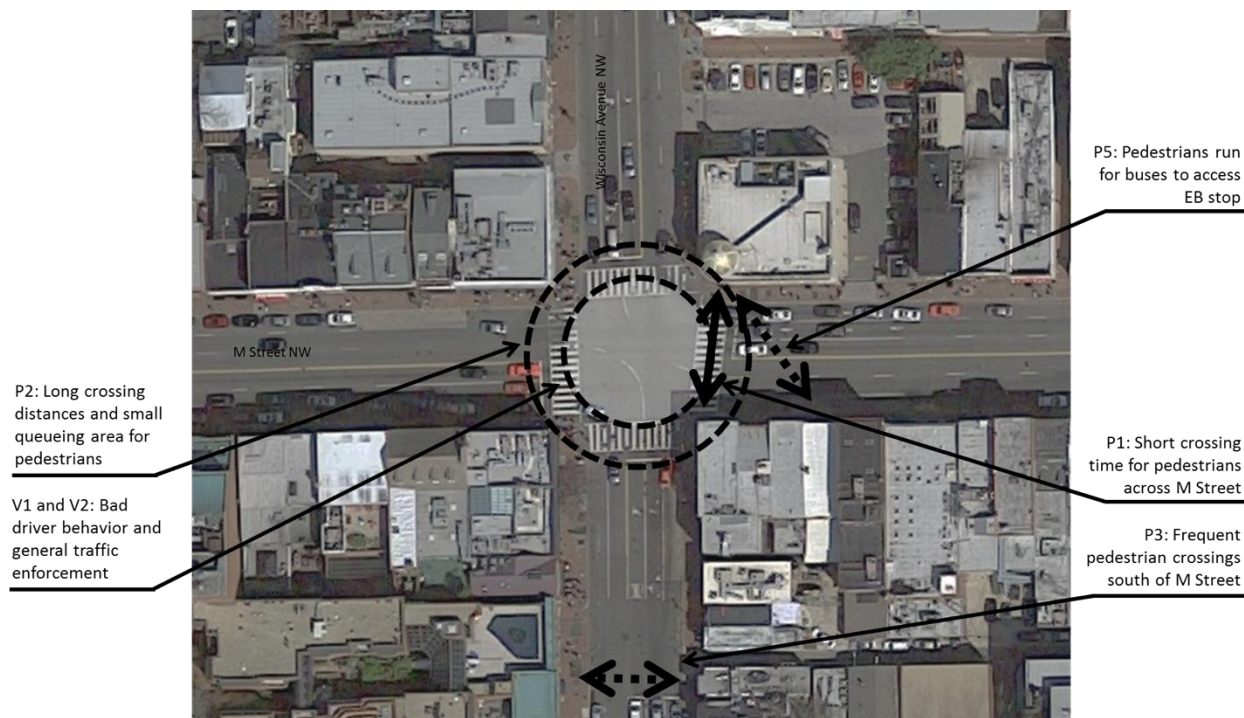


Figure 5: M Street and Wisconsin Avenue NW Site Visit Observations

1.5. Next steps

Issue	Resolution	Status	Timeline
Pedestrian Issue			
P1. Short crossing time for pedestrians across M St.	Signals redid overlap and increased Flashing Don't Walk times.	Signals redid overlap and increased Flashing Don't Walk times.	Implemented 3/31/16
P2. Long crossing distances and small queuing areas for pedestrians, especially on weekends.	Curb extension not feasible due to eastbound right turning vehicle path impact on northbound left turn lane.	Not recommended for implementation.	Resolved 7/25/16
P3. Frequent pedestrian crossings on Wisconsin outside of crosswalk south of M Street.	Crosswalk recommended but will require ADA ramps.	To be scheduled for implementation.	Implementation Summer 2017.

Issue	Resolution	Status	Timeline
P4. High pedestrian volumes coincide with off-peak auto times.	Signals redid overlap and increased Flashing Don't Walk times.	Evaluation completed. Coupled with signals evaluation for Issue P1.	Implemented 5/31/16
P5. Pedestrians run for buses, particularly for buses accessing eastbound M Street stop east of Wisconsin Ave.	Evaluate potential for bus stop location changes.	Safety and operations evaluation recommends moving bus stop at SE corner of Wisconsin/M to M/31st.	Bus stop relocation to be coordinated with WMATA. Timeline not set.
Vehicle Issues			
V1. Bad driver behavior, particularly running red lights and blocking box.	Crash history and safety analysis does not fit with need for red light camera. Not priority installation location currently.	MPD has determined a red light camera is technically feasible, but would have substantial costs that are not budgeted at present.	Re-evaluate need based on available resources.
V2. Traffic and turning movement volumes.	TCO deployment under investigation.	Signal timing evaluation completed. Data collection took place June 14-15. TCOs began deployment.	Implemented 7/16/16

Table 6: M Street and Wisconsin Avenue NW Next Steps

Conclusion

DDOT continues to place a high priority on resolving the issues disclosed during the site visit process. The agency has found the exercise to be a highly valuable complement to its ongoing Vision Zero activities. The next round of site visits has been completed, and DDOT has begun implementing recommendations and is finalizing a report to issue in January 2017.